

CONSTRUCTION MANAGEMENT PLAN



Wednesday 4th March 2020

CLIENT - MORGANS MANAGEMENT

PRODUCED BY - LONDON COST CONSULTANCY

Revision - *

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1.0 Introduction

This Construction Management Plan has been produced by London Cost Consultancy Limited for the demolition and re-construction of the existing two storey building to achieve a like for like replacement. The new construction would match the existing in terms of appearance, massing and style. The first aspect of the works will provide the opportunity to improve and reconfigure the existing residential accommodation. However, the works will not increase the number of residential units within the like for like massing. The second aspect of the proposed works involve the extension to the existing massing building by three storeys to create an additional 7 high quality residential units. These steps will enable the new construction to meet modern day building & safety standards, whilst also providing the necessary structural support for the new extension levels.

This construction management plan has been prepared to outline the approach to managing the building works on site and ensuring that these works cause minimum disruption to the neighboring properties. Furthermore, the exact construction dates for each of the construction phases is not yet known. This Plan will therefore be revised upon receipt of any planning comments and the appointment of the main Contractor.

2.0 Construction Management Plan Objectives

The purpose of this Construction Management Plan is to provide the planning department with information concerning the following items:

- The Plans should provide details on how construction work (Including demolition) will be undertaken in a manner that disruption to traffic and pedestrians on Brent Street and the roads surrounding the site is minimized.
- Provide details how construction vehicle movements will be carefully planned and coordinated to avoid the AM and PM peak periods.
- Logistics and site set up for the construction.

This Construction Management Plan has been prepared with the intention of seeking approval for the site prior to commencement.

3.0 The Site

3.1 Existing Site

The site is located on the corner of Avenue Rd and Crescent Rd in the Crouch End area. The site slopes up over an 8m change in level from street level to the top of the site to the South. Multiple storey apartment buildings lie to the East and West of the site, and victorian style dwellings to the north along Avenue Road. To the South of the site there is a public parkland walk linking Highgate Tube station to Finsbury Park in the East.

The existing building was commissioned by local MP Eleanor Rathbone and was originally designed as a retirement home for survivors of World War 2. It now functions as a series of private apartments.

The building is formed of two wings the first of which being the front 2 storey structure containing 10 units which vary in size from studio to two bedrooms. The second structure on the site is the dominant 12 storey high-rise which houses 48 apartments.

Currently there is a series of unassigned parking bays for free use by the residents of the apartments. There is ample private amenity within the site boundary with a large south facing garden for the residents.

Previous planning permission was granted in 2003 to alter the front wing of the building. This permission covered internal alternations to the layout, external alteration to add a new awning, paint the walls and add a new roof-light (HGY/2003/1853).

Planning permission has also been granted for the demolition and re-construction of the front building to site to which this Construction Management Plan relates to (HGY/2018/0589).

The site is not located in a conservation area, however the Crouch End Conservation Area is located directly north of the site situated above Avenue Road.

3.2 Proposed Scope of Works

The scope of work for 96-101 Brent Street includes the following;

- Demolition of existing building to the front of the property
- Temporary works to allow for existing tower block residents to gain safe access
- Sub-structure works including Groundworks and Foundations
- Concrete framed construction of new five storey building, including slabs and columns.
- Closure and waterproofing of the concrete frame
- Full fit out of 17 Residential Units

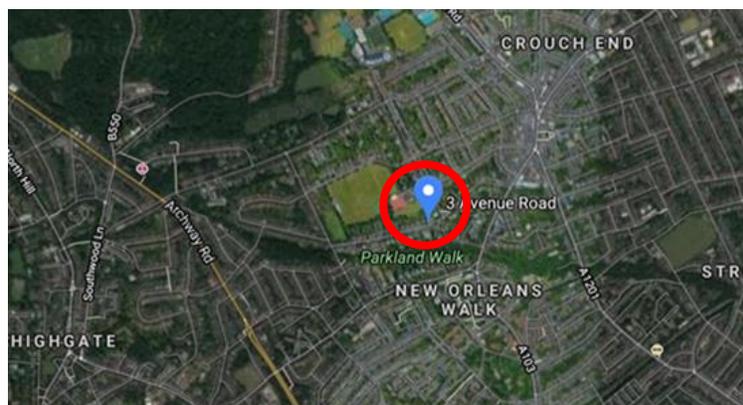
3.3 Site Logistics and Set Up

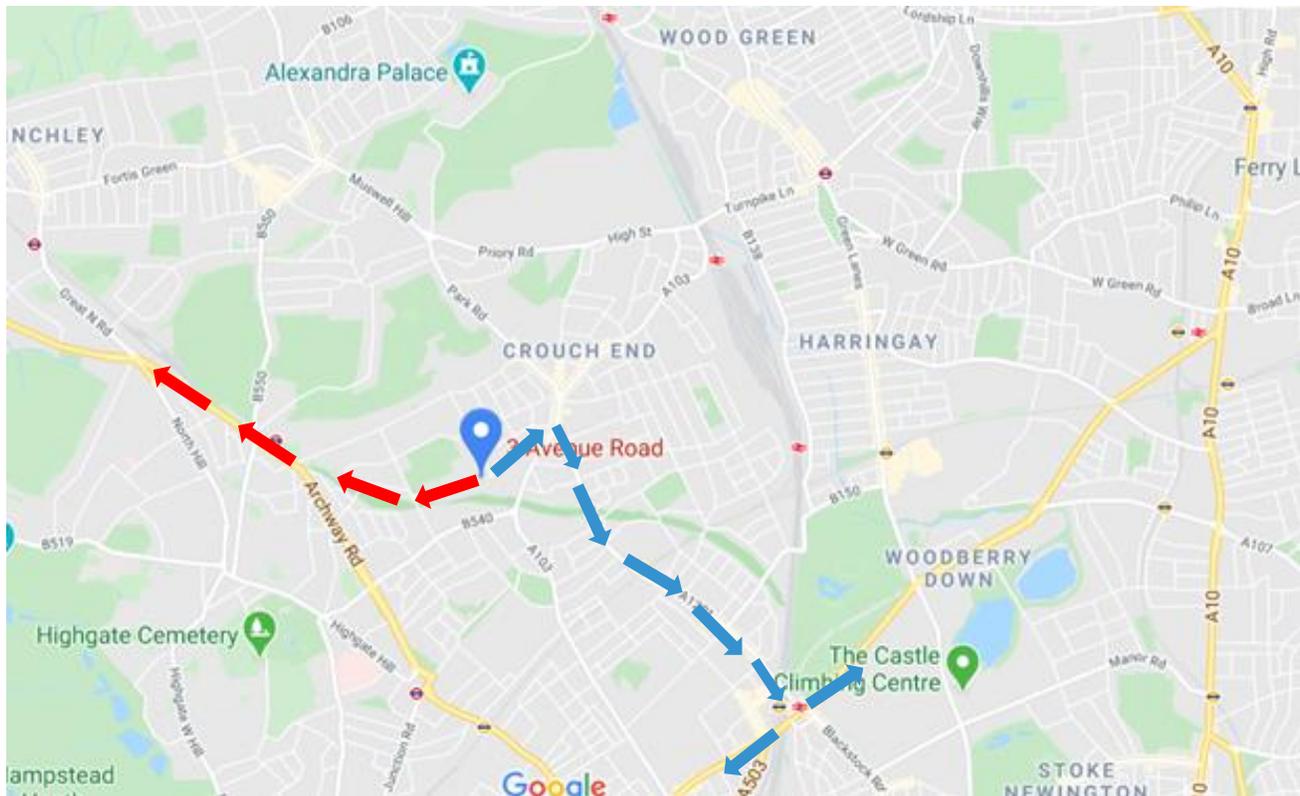
An Outline Site Logistics Plan has been enclosed under Appendix A indicating proposed site set up, access into site, delivery areas, hoarding etc. It will be the main contractor's responsibility to apply for any Public Highway hoardings that are necessary for the completion of the works and safety of the public. These will be applied for prior to works commencing. However, owing to the nature of the site all hoarding should fall within the site boundary.

3.4 Size, Nature, Issues and Challenges of the Works

The size of the site at its widest points is 63 meters by 70 meters. However, the proposed re-development building is 42 meters by 12 meters. The structural and envelope construction will be a concrete pour to create the external envelope/frame with rainscreen cladding on the upper three floors as per the planning approval. Small cranes may be required for this development and necessary licenses will be sought by the appointed main contractor. As the works are wholly within the site boundary and set back from the main road this reduces risk and disruption to the street/neighbouring properties. In general terms, the concrete pour will be completed with commencement from the north of the site to the south.

Deliveries to the site will be via Avenue Road and it will be within the main contractor's remit to provide a banksman and traffic marshals at times of deliveries. The number of deliveries to the site will be in the approximate region of 142 (based on 4 per week). Delivery's will be recorded in a log by the Main Contractor to ensure they are planned to cause less disruption to the street and neighbouring properties. It will be the Main Contractor's responsibility to ensure all times of deliveries are outside of peak hours/busy traffic periods along Avenue Road. Vehicular Access to and from site is available to the east and the west. This site is in the heart of Highgate and provides connection with retail facilities. To the west of the site, within 750meters, is the A1 (Archway Road) providing access connections to motorways etc. Please also note the maps below highlighting access/egress routes.





Works to the facades of the property are currently programmed to be completed without the use of scaffolding. Subject to the main contractors' comments at procurement of this package, this will confirm whether scaffolding is required. Should this be necessary to erect scaffolding, this will be designed, and licenses sought if applicable. The design of the scaffold will be confirmed prior to erection.

The yield area for storage of materials will be contained wholly within the site. Thus, allowing quick turnaround for deliveries. This area has been indicated on the Site Outline Logistics Plan under Appendix A.

All works will be completed in accordance with Best Practice and in consultation with neighbours to minimize impact for any distribution. The Considerate Contractors Scheme is to be adopted for this development and this is further detailed later on within this Construction Management Plan.

3.5 Potential Receptors Likely To Be Affected/How This Is To Be Reduced

Neighbouring properties that potentially may be affected by the works are Christopher Lodge and 1 Avenue Road.

A complaints book and signing in / out book will be utilized on site also to keep records of staff on site and also any complaints that neighbours / members of the public may have. The record of complaint should include the name of person wishing to issue the complaint, the date, the time, the nature of the complaint, and remedial action required to resolve the issue. This record should be regularly reviewed, and any complaints dealt with and resolved promptly in order to keep disruption to a minimum and keep good neighbouring relations.

The main contractor will manage all activities on site to reduce disturbances to the neighboring properties. Therefore, to reduce dust and dirt on the public highway to a minimum we will allow for regular cleaning and wheel washing facilities on site. These are to be used on any vehicle that visits or leaves site in order to minimize the debris in Avenue Road and the surrounding roads. Wheel washing facilities are to discharge into the existing mains sewer via the drainage on site. Where necessary Main Contractor will employ road sweepers to control excess debris.

Dust will be a major factor during the demolition of the existing double storey building and the levels of dust emitted will be monitored at regular intervals throughout the works. In order to minimize these levels, the contractor will spray a fine water spray onto the materials prior to demolition as this will dampen the material and cause less dust fibers upon removal.

The Considerate Contractors Scheme will be adopted for the entirety of works on site. Contact details will be affixed to the front site hoarding indicating details of who to contact. This contact list will include names and numbers of staff on site should neighbours/the public require the need to contact a staff member.

3.6 Hours of Work

In order to reduce the noise disturbance to neighboring properties we propose, in accordance with the approved planning permission, all building works can only occur between:

- o 08:00 and 18:00 Monday to Friday;
- o 08:00 and 13:00 on Saturday; and
- o Not at all on Sundays, bank holidays and public holidays, unless agreed otherwise with the LPA.

3.7 Surveys and Services

All local services have been contacted prior to this CMP being completed.

All services to the site are to be upgraded with additional connections and runs as required in accordance with the relevant legislations.

An Arboricultural Surveyor has been appointed to outline useful information on the trees so that property managers can make informed decisions. Based on the information that they receive from the survey; they may then decide what to do with the trees. This is also in line with the planning conditions from the granted planning consent.

A Management asbestos plan is currently in place for the property so we know there is Asbestos on the site however prior to any demolition, a demolition survey and reports will be issued. A portion of the building finishes contain asbestos and necessary actions to be taken to remove these. This will be adopted by a contractor licensed by the Health and Safety Executive prior to any other works/demolition starting in these locations. Certification will be obtained following safe removal from site.

4.0 Proposed Programme

Proposed Site Construction Commencement date – Monday 5th October 2020

Proposed Site Construction Completion date – Friday 4th February 2022

Total Duration Period – 65 weeks plus Christmas shutdown

Please note Gantt charts/precise programmes are currently unavailable as trade packages are currently in design/tendered awaiting trades to confirm programme of works. Enclosed under Appendix B is the anticipated milestone programme of works for the development including enabling works, piling, structure, roofing works and fit out of the apartments.

5.0 Site Contacts

Site Address: Avenue Heights, 5 – 7 Avenue Heights, Highgate, Haringey, London N6 5DS

Planning application reference: HGY/2018/0589

Type of CMP – Planning Approval

Main Contractor – TBC

Emergency Contact – TBC

Community Liaison Contact – TBC

6.0 Transportation Issues

6.1 Access, Vehicles and Timings

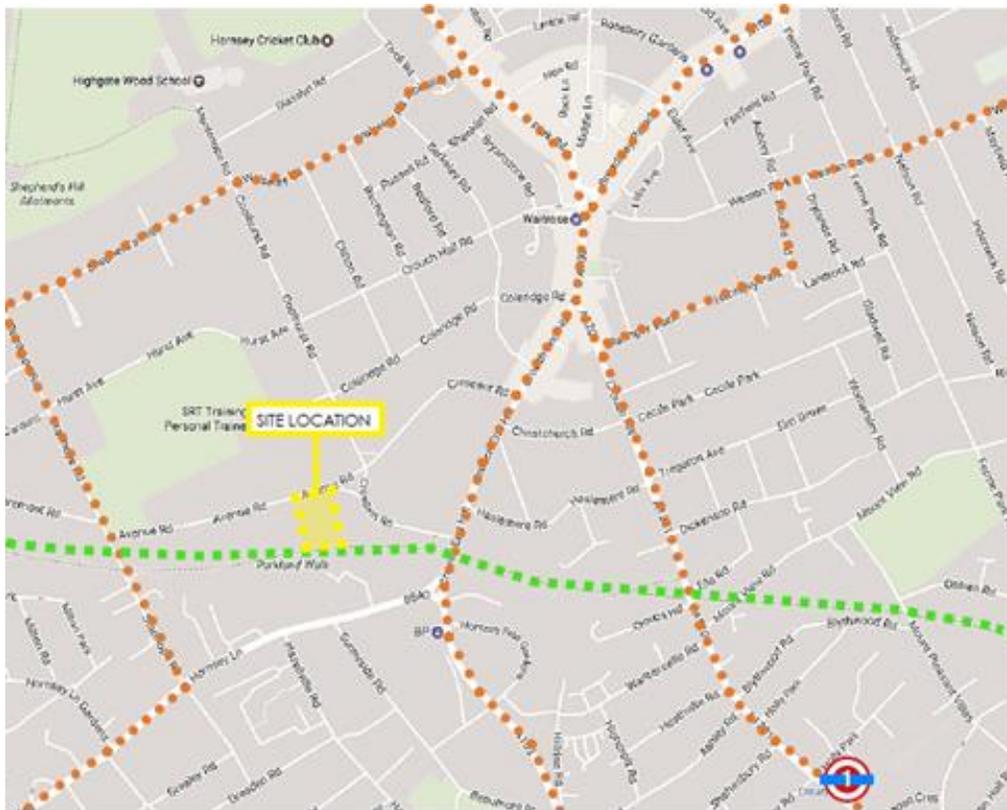
The access for vehicles on site will be from Avenue Road and all trade contractors/suppliers will be required to give a notice period of 48 hours prior to delivery. The movement of materials will also be controlled by the project manager. They will be responsible for the coordination and control of all aspects of material deliveries and movement. All loading and unloading of plant/materials on site is to be controlled by the project manager. Timings for delivery's will be in accordance with the working hours set for the site however it will be the trade contractor's responsibility to ensure all times of deliveries are outside of peak hours/busy traffic periods along Avenue Road (please refer also to sections 3.4 and 3.6).

It is anticipated vehicles attending the site for deliveries or collections will consist of:

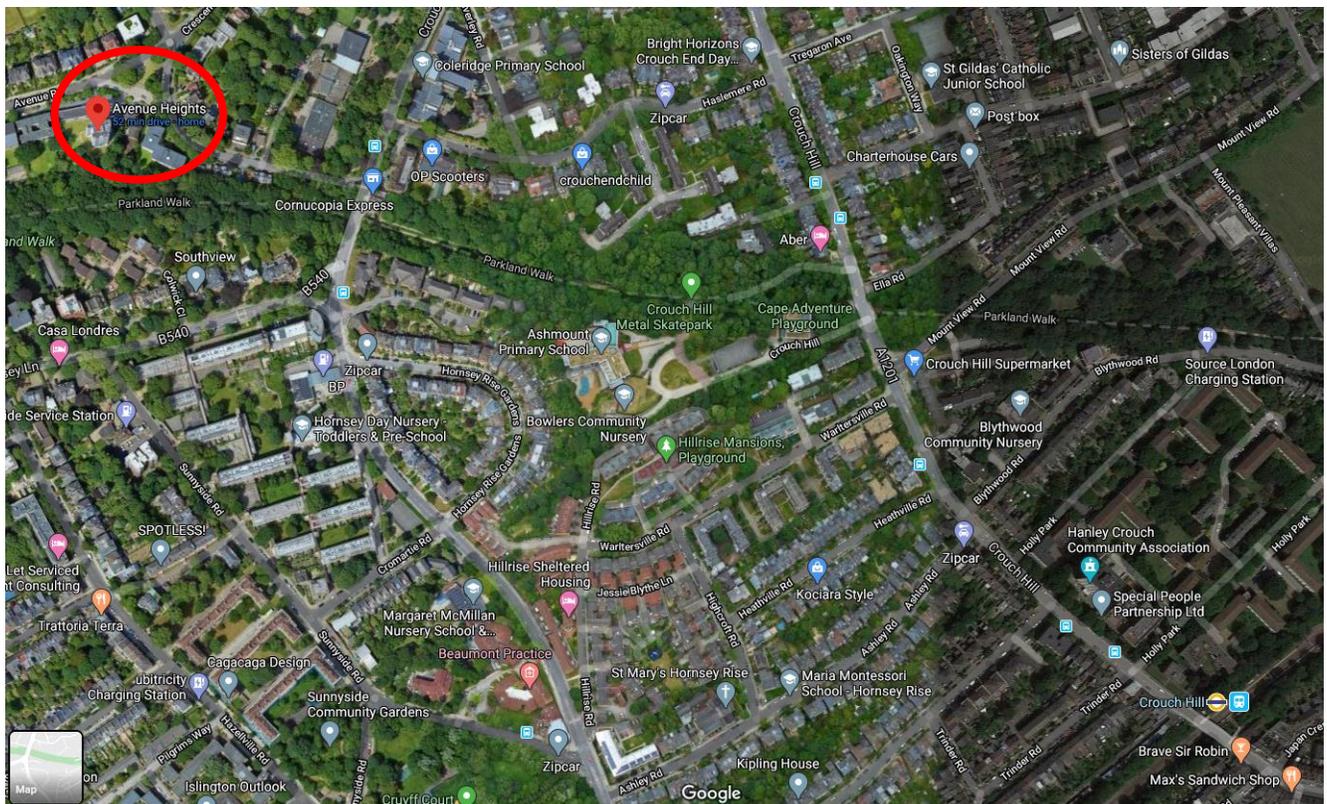
- Skip lorries for general building waste removal (including concrete wash out skips)
- Concrete trucks – maximum pours will be circa 100m³, i.e. 15 to 16 wagons over 6 to 8hour period, 1 per 30 mins. There will be approximately 900m³ of concrete pour on this development site.
- Steel reinforcement deliveries – brought in to suit concrete works. Anticipated the delivery's will be on smaller 15t rigid lorries.

Other deliveries include cladding, insulation, block work, roofing and internal fit out materials including MEP equipment. The main contractors site team will have to ensure that suppliers, as part of their delivery notification, confirm the load arrangements to ensure minimal dwell time on site. Trade contractors will be notified of the local area and public transportation to site to minimize the vehicle activity thus reducing impact on the neighbours.

The site is located in close proximity to Highgate Underground Station. Located approximately 1km to the west of site, approximately 15 minutes walking distance. The station provides access to the London Overground thus will provide good access/egress from site for all contractors.



-  Crouch Hill Station
-  Bus Routes
-  Parkland Walk



6.2 Temporary Structures on The Public Highway

The requirement of scaffolding the front façade on Avenue Road, if required, is going to be within the sites boundary and offset from the road by approximately 10 meters. Therefore there is no overhanging or encroachment onto the existing footpath. Should scaffolding be required, safety measures will be carried out to protect all persons passing the site. The footpath along Avenue Road requires a width of 2 metres at all times and therefore this will be achieved. Subject to procurement and appointment of a main contractor correct safety measures will be carried out to protect all persons on and off the site. This will be designed prior to construction and will be subject to agreement with local authorities where applicable. Licenses will also be placed on the scaffold as required.

It is anticipated that the scaffold will be enclosed with a 2.4m high hoarding to prevent and unauthorized persons entering the site and tampering with the fully fixed scaffolding.

Temporary site porter cabins will also be proposed within the site to provide welfare, meeting rooms etc. this again is due to the nature of the proposal and access on site. These will be a temporary measure and welfare areas etc. These will be removed when site welfare can be established within the proposed build.

6.3 Vehicular Movements and deliveries

The following list provides detail of the type of vehicles that will need to gain access to the site during the construction process.

- Tipper Lorry 8 Wheel, 20 Tonne, G.V.W
- Concrete Delivery Vehicle 6 Wheel, 24 Tonne, G.V.W
- Building Panel Deliveries 4 Wheel, 17 Tonne, G.V.W
- Ballast and Loose Materials 4 Wheel, 17 Tonne, G.V.W, Tipper
- General Building Materials 4 Wheel, 17 Tonne, G.V.W, HIAB Flat Bed
- Wheeled 360 Excavator, 23 Tonne GVW

Vehicle Movements			
Work Phase	Duration	Anticipated number of trips daily	TOTAL (based on 5 day weeks)
Mobilization Period	4 weeks	0.50	2.00

Site Set-Up and Hoarding	1 week	0.50	0.50
Demolition	6 weeks	6.00	36
Foundations and ground works	6 weeks	2.00	12
Concrete Frame and Pour	15 weeks	2.00	30
Façade	12 weeks	1.00	12
Roof	7 weeks	1.00	7
M&E	28 weeks	0.75	21
Internal Fit-Out	28 weeks	0.50	14
Metal works & Balcony	5 weeks	0.50	2.5
External Works	6 weeks	0.50	3
Commissioning/Snagging	6 weeks	0.33	1.98
Totals (deliveries will interlink as programme)	124 weeks	15.58	141.98

The previous table highlights vehicular movements for delivery's/removal of waste on site on a daily basis. This has been calculated based on the project however this is an estimate based upon the programme. The trade contractors appointed for the project will confirm programme, phasing, delivery's, waste removal etc. upon their appointment. For the site a Traffic Management system will be in place with only one 1 HGV on the site at any time and HGV Vehicles will be accompanied by a Banksman to the site from Avenue Road. In terms of hourly delivery's/waste removal the maximum number will be 2 trucks per hour.

Delivery vehicles will access the site via Avenue Road – see Site Layout Plan. Site logistics and construction phasing will be developed to ensure that access to Avenue Road/surrounding roads is maintained at all times. It will be managed on site that all deliveries are completed on a 'just-in-time' basis. Deliveries will be carefully planned, pre-booked and managed on site to ensure no back up of vehicles on Avenue Road and the surrounding areas and timed to minimize disruption to local residents and neighbours.

6.4 Parking Suspensions/Road Closures

Due to the nature of the site it is anticipated there will be no requirements for road suspension for this project. The site at present has resident permit holder parking bays in the road directly in front of the site and along Avenue Road. Parking bay suspension is deemed as non-applicable as well as road closures. Should this change once the main contractor is appointed licenses would be sought. Due to the kerbs distance from site it is unlikely that the contractor will require the closure of the footpath for this development. This will therefore mean less disruption to Avenue Road and enhanced safety for pedestrians.

6.5 Abnormal Delivery's

The materials proposed on site are standard materials for a building project all to be on a just in time delivery basis. There are no abnormal deliveries planned for this project based upon the materials proposed.

7.0 Environmental Issues

7.1 Noisy Operations

Due to this being a proposal for demolition and construction of a new build concrete pour development, the majority of the works are noisy operations. In order to reduce the noise disturbance to neighboring properties it is proposed, subject to planning permission, all building works will only occur between:

- o 08:00 and 18:00 Monday to Friday;
- o 08:00 and 13:00 on Saturday; and
- o Not at all on Sundays, bank holidays and public holidays.

Noise levels will be monitored constantly during construction and any complaints received will be responded to immediately. Notification to local residents will also be given in advance.

7.2 Noise and Vibration Levels

Sources of noise/vibration will generally be as follows: -

- Excavation.
- Demolition of existing structures.
- Pokers during concrete placements.
- Hand tools for fixing of cladding and the like.
- Hand tools for internal fit out.

Mitigation measures will be incorporated during the construction/demolition works to prevent noise and vibration disturbances from the activities on the site. All works that produce noise/vibration that could cause disturbances during this demolition proposal will be carried out a minimum of 24 hours after neighbour consultation. All works will also be within the hours described in section 4.4 thus reducing the disturbances caused.

8.0 Monitoring, Compliance, Reporting and Consultation about Traffic and Activities related to the Site

In addition to the measures previously explained in this CMP, the Main Contractor will be required to allow for all forms of monitoring compliance, reporting and consultation. As London Cost Consultancy will be tendering the proposed works, we can confirm that the main contractor on the site will be required to have the latest and correct training including all certification prior to them working on the site.

All vehicular movement to and from the site will be managed by the project manager. Notification will be given to all contractors/sub-contractors when vehicles travel to site and any unloading will be completed by all skilled contractors in order to ensure no vehicles dwell on site.

Community consultation meetings with neighbours, ward councilors, local businesses etc. has been carried out for the proposal as part of the planning approval process. Comments have been replied to and the proposals will be as per the planning application/conditions set out. The site has previously had community consultations with comments incorporated into the overall proposed scheme. A complaints book and signing in / out book will be utilized on site also to keep records of staff on site and also any complaints or comments that neighbours / members of the public may have. The record of complaint should include the name of person wishing to issue the complaint, the date, the time, the nature of the complaint, and remedial action required to resolve the issue. This record should be regularly reviewed, and any complaints dealt with and resolved promptly in order to keep disruption to a minimum and keep good neighbouring relations.

9.0 Waste Management

All waste materials from the site will be transferred to skips located within the sites boundary. In order to minimize the dust all materials will be dampened prior to the demolition and insertion in the skip. The skip should be covered with tarpaulin when not in use to reduce the disturbance to neighbouring properties and the public.

The site will also be kept clean with the storage of materials to be within the sites boundary. Storage of materials will be controlled by the deliveries to the site, with each delivery being to a programme of progress thus minimizing the amount of congestion within the storage areas on site. The project manager will ensure that all access routes, fire escapes and staircases are swept and kept clear from debris regularly to keep a high level of health and safety on site. All other areas will be swept clean on a weekly basis.

The re-cycling of materials will be subject to each relevant trade contractor and how they are intending to dispose/manage the waste materials created during the construction period on site. Therefore, waste management plans will be completed by each relevant trade contractor explaining these items. Where materials on site cannot be re-cycled at construction stage on site, to maximize the potential of re-cycling, they are to be dealt with off-site to reduce the impact of noise and dust. All re-cycling and waste materials removed from the site is to be taken to authorized waste disposal or waste treatment sites. Following the removal of all waste on site including delivery vehicles to and from site, the road and pathways will be checked to ensure no debris is left and where required cleaned to minimize disruption.

Where hazardous waste or substances are to be removed from the site, they will be removed/disposed of by an appropriately licensed waste disposal operator.

10.0 Risk Analysis

All construction works on site carry hazards and for this site the following have been identified for the construction works. These hazards will be assessed throughout the construction period on site and preventative measure will be in place to ensure the overall potential risk is reduced as low as possible.

- Demolition Works
- Ground Works
- Scaffolding works
- Concrete frame construction
- Working at height
- Protection from falling
- Noise, Dust and Vibration control measures
- Traffic management and deliveries

11.0 Site Security and Safety

Signage specifying any security measures and key contact details shall be erected on the perimeter of the building site (i.e. attached to the building, fence or hoarding). A 24hour contact name and mobile phone number will also be provided.

The site area and the contractor's compound, will be enclosed with a 2.4m high timber hoarding. This will be adapted as necessary as works progress and will be painted. The main contractor will be responsible to ensure all hoarding panels are maintained and kept clean for the duration of the project.

Access to the welfare areas will be from the site as to assist with security and all areas of the site are to be securely locked when the site is not in use.

12.0 Details of How Pedestrian and Cyclist Safety Will Be Maintained

When vehicles are entering or leaving the site, there will be a banksman/traffic marshals supervising Avenue Road and the surrounding roads. This will also be applicable when deliveries or removals from site occur.

The general public/pedestrians will have right of way along the pathways along Avenue Road and the surrounding roads. The construction site gates will be kept closed and monitored by the site manager, only when deliveries are made to the site will they be opened to allow vehicles onto the site, at which time contractors presence or barriers will be put across the pavement to prevent access by pedestrians. All delivery vehicles will be supervised/controlled by the banksman.

It will be the site manager's role to ensure all external perimeters of the site is regularly checked to ensure that any debris is kept clear of the roads.

Cyclist safety will be completed in a similar way and when there are any deliveries or loading vehicles within the delivery area will be coned off to direct the cyclist around the lorry.

13.0 Site Drawings

Should any additional drawings be required, please contact either London Cost Consultancy Ltd or the Architect directly.

Signed: 

Date: 18/07/2019

Print Name: Nick Wells

Position: Quantity Surveyor

APPENDIX A –
Outline Site Logistics Plan
(London Cost Consultancy Ltd)

APPENDIX B –
Anticipated Programme of Works
(London Cost Consultancy Ltd)

Appendix C –
Site Location Plan
(21st Architecture Ltd)